

CITY OF SAN DIMAS DOWNTOWN SPECIFIC PLAN

COMMUNITY MEETING #4 // MARCH 29, 2023



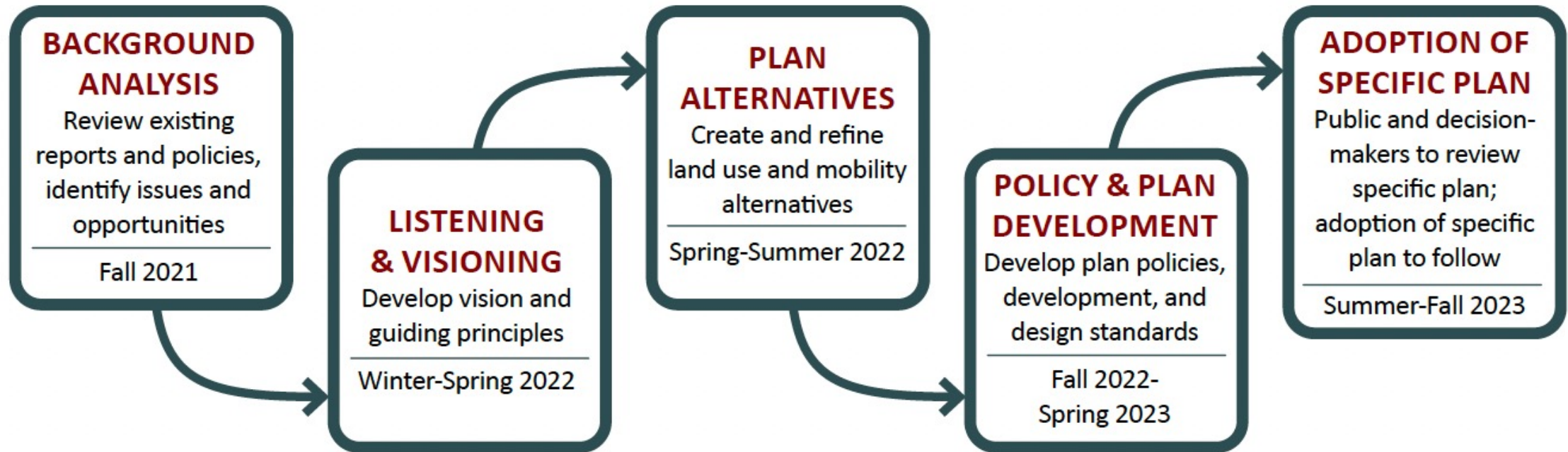
MEETING AGENDA

- I. Welcome and Update
- II. Project Timeline
- III. Summary of Public Input Received at Community Meeting #3
- IV. Draft Land Use Plan, Land Use Table, Development and Design Standards/Guidelines
- V. Opportunity Sites - Draft Concept Plan
- VI. Draft Mobility Strategies
- VII. Stations to Visit
- VIII. Adjourn



PROJECT TIMELINE

TIMELINE OF SAN DIMAS DOWNTOWN SPECIFIC PLAN COMPLETION:



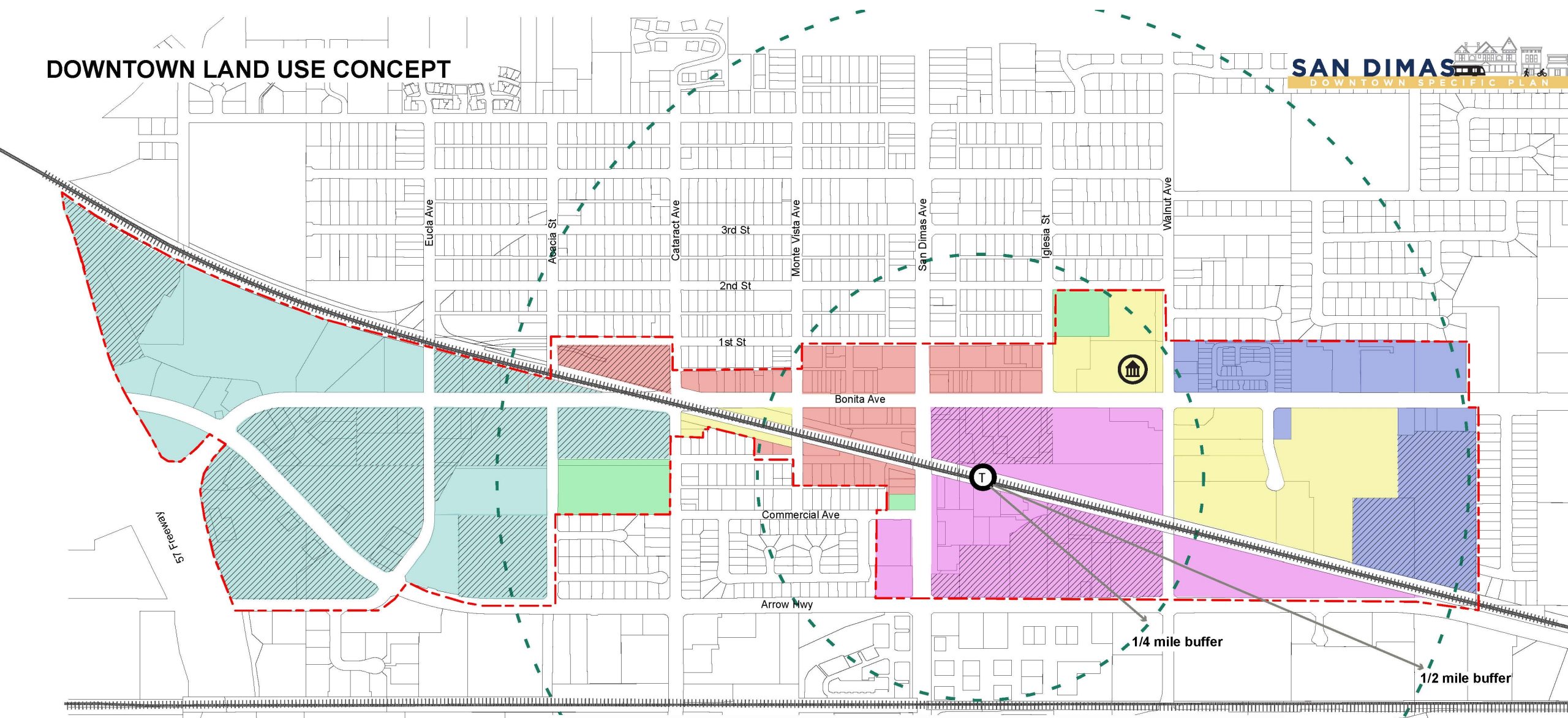
PUBLIC INPUT RECEIVED AT COMMUNITY MEETING #3

- ➔ **On Draft Land Use Concept Plan with written narrative for each plan area:**
 - ➔ Generally comfortable with plan areas as presented
 - ➔ Desire for more community amenities- public and open spaces for gathering, public art, historic preservation elements in public space
 - ➔ Desire for more restaurants and activity/nightlife-supporting businesses
- ➔ **On Mobility Linkages - Opportunities and constraints, including options for improvements:**
 - ➔ Desire for pedestrian improvements and enhancing pedestrian safety/slowing vehicles on Bonita Avenue
 - ➔ Desire for improved bicycle infrastructure, but majority wanted bike lanes located on other streets than Bonita Avenue
 - ➔ Desire to connect “new” (west) and “old” (east) downtown through streetscape improvements
 - ➔ Planning for parking a high priority

DRAFT LAND USE PLAN, LAND USE TABLE, DEVELOPMENT AND DESIGN STANDARDS



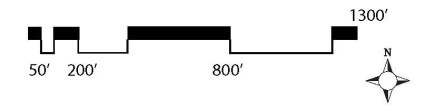
DOWNTOWN LAND USE CONCEPT



LEGEND

- - - Specific Plan Area Boundary
- Gold Line Route
- Transit Station Platform
- City Hall
- Gateway Village - West
- Gateway Village - East
- Town Core
- Transit Village
- Public/Semi-Public
- Open Space
- Housing Element Opportunity Sites

The Housing Element Opportunity Sites represent the sites that the City has identified in the Housing Element Update to be available to meet the City's share of the region's need for housing, by providing a requisite type of development (multi-family or mixed-use), density, and affordability mix of housing at each of these sites.



LAND USES AND SECTION OF DRAFT LAND USE TABLE

Use Table Key - Entitlement Options	
Permitted	P
Minor Use Permit	MUP
Conditional Use Permit	CUP
Upper Floor / Not on Key Street Frontage Use. (Addendum to above classifications indicating use is allowable on the second story or above, or on the ground floor if not abutting Bonita or San Dimas Avenue street frontages (NKSF = "Not Key Street Frontage"))	(2/NKSF)
<i>See SDMC 18.196 for Temporary Uses</i>	

San Dimas Downtown Specific Plan Land Use Districts - DRAFT						
	Gateway Village West	Transit Village	Gateway Village East	Town Core	Public/Semi Public	Open Space
Commercial Uses						
Retail and Service						
Restaurants, Sit Down	P	P	P	P	-	-
With Alcohol Service	MUP	MUP	MUP	MUP	-	-
With Live Entertainment	MUP	MUP	MUP	MUP	-	-
Retail Pharmacies	P	P	P	P	-	-
Food Halls and Markets with a minimum of 75% of floor area dedicated to food-related uses	MUP	MUP	-	MUP	-	-
Specialty Food Stores (including but not limited to coffee shops, ice cream stores, juice bars, and similar)	P	P	P	P	-	-
Studios (including but not limited to dance, gymnastics, health and exercise, martial arts, music, personal training, photography, pilates, yoga and similar) which are up to a maximum size of 5,000 square feet of gross floor area	P	P	P	P	-	-
Studios (including but not limited to dance, gymnastics, health and exercise, martial arts, music, personal training, photography, pilates, yoga and similar) which are up to a maximum size of 5,000 square feet of gross floor area and when located within a mixed-use development project and adjacent to residential uses	MUP	MUP	MUP	MUP	-	-
Specialty Retail Stores (including but not limited to appliances, books, clothing, furniture, hardware, home goods, jewelry, music, sporting goods, toys, and similar)	P	P	P	P	-	-
Recreation, Education, and Public Assembly						
Childcare and Adultcare Centers	-	-	MUP	-	-	-
Commercial Recreational and Social Experiences, Indoor (including but not limited to art courses, bowling alleys, cooking classes, escape rooms, laser tag, trampoline parks, and similar)	P	P	P	P	-	-
Entertainment and Performance Establishments (including but not limited to live performing arts theaters, concert halls, comedy clubs, movie theaters, and similar)	CUP	CUP	-	CUP	-	-
Museums, Art Galleries and Working Studios, and similar Cultural Institutions	P	P	P	P	P	-

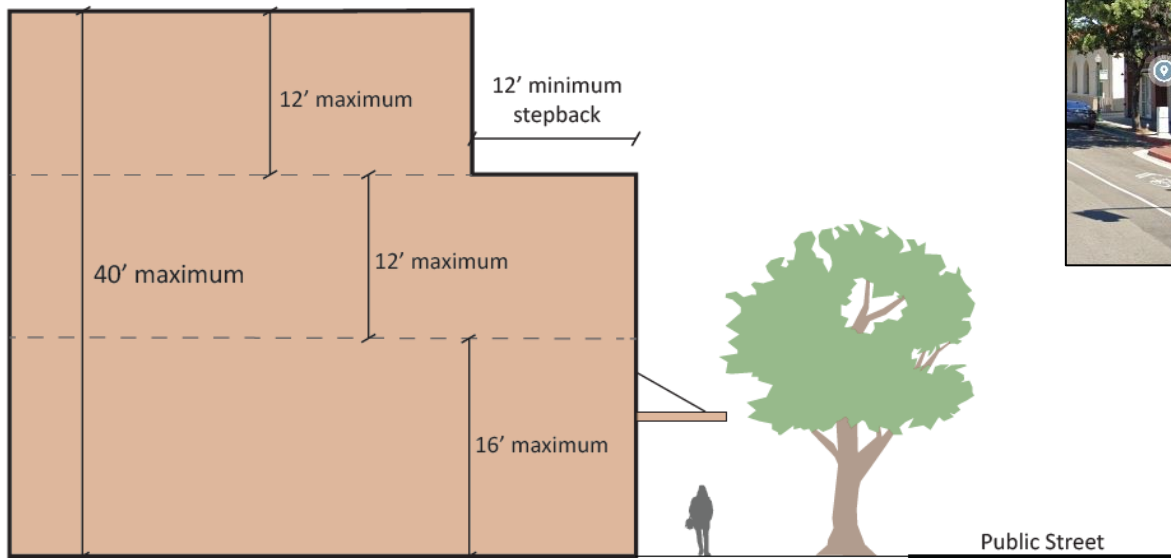


SECTION OF DRAFT DEVELOPMENT STANDARDS TABLE

Standard	Gateway Village West	Transit Village	Gateway Village East	Town Core
Scale				
Allowable Density				
Maximum Dwelling Units per Acre	45	40	35	30
Allowable Intensity				
Maximum Floor Area Ratio	2	1.8	1.8	1.5
Maximum Number of Stories Above Grade				
Mixed Use (Commercial Ground Floor)	4 stories	4 stories	3 stories	3 stories
Multi-Family Residential (Residential Ground Floor)	4 stories	4 stories	3 stories	3 stories
Commercial	2 stories	2 stories	2 stories	2 stories
<i>Exceptions Provided for Developments Which Provide Additional Public Parking or Amenities- Reference Section 4.3</i>				



STEPBACK EXAMPLE— 3 STORIES ADJACENT TO PUBLIC STREET



DESIGN STANDARDS AND GUIDELINES

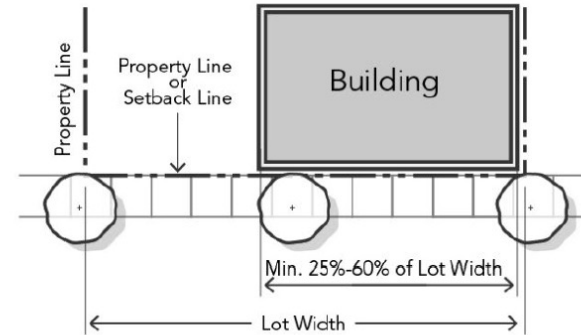
- ➔ Establish the design for new buildings or adaptive reuse of existing buildings
- ➔ Design standards are mandatory requirements that must be satisfied in project design
- ➔ Objective design standards included to meet recent state law for eligible housing
- ➔ Design guidelines will also be included for design objectives city wants to see, but not mandatory



DESIGN STANDARDS AND GUIDELINES

- ➔ Topics to be addressed:
 - ➔ Building Placement and Orientation
 - ➔ Pedestrian Circulation and Access
 - ➔ Vehicular Access
 - ➔ Building Massing, Form, and Scale
 - ➔ Building Articulation
 - ➔ Materials and Finishes
 - ➔ Rooflines
 - ➔ Entries, Doors, and Windows

Building Placement



Design Standards and Guidelines

➔ Topics to be addressed (continued):

- ➔ Lighting
- ➔ Signage
- ➔ Awnings
- ➔ Landscaping
- ➔ Public Open Space
- ➔ Walls and Fences
- ➔ Mechanical Equipment
- ➔ Sustainable Building Design



OPPORTUNITY SITES – DRAFT CONCEPT PLAN



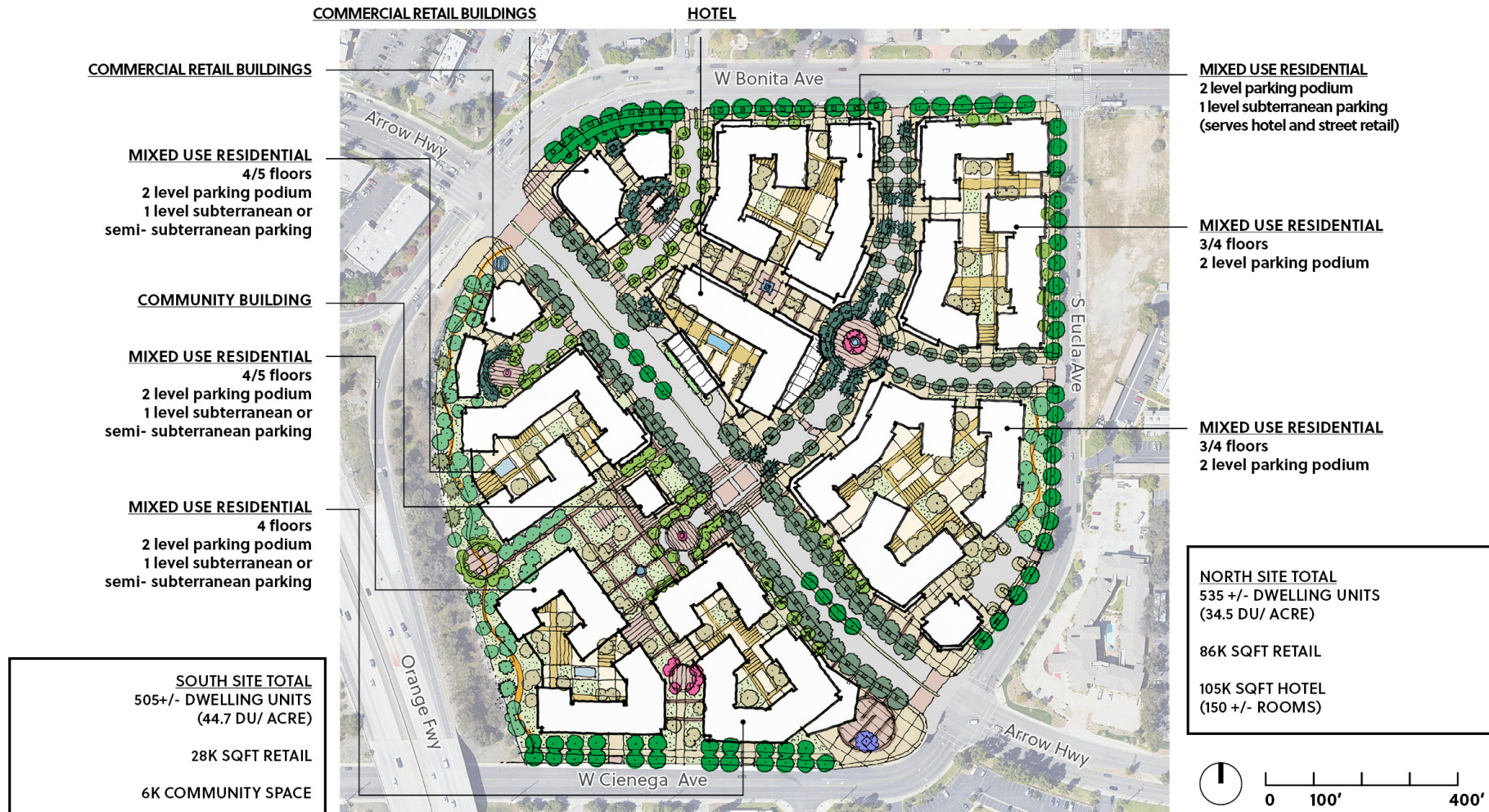
OPPORTUNITY SITES

- ➔ San Dimas Station North and South Sites, which were identified as Housing Element opportunity sites
- ➔ Concept illustrations show a vision for what could occur in the area in the future
- ➔ New development would be built and phased over many years



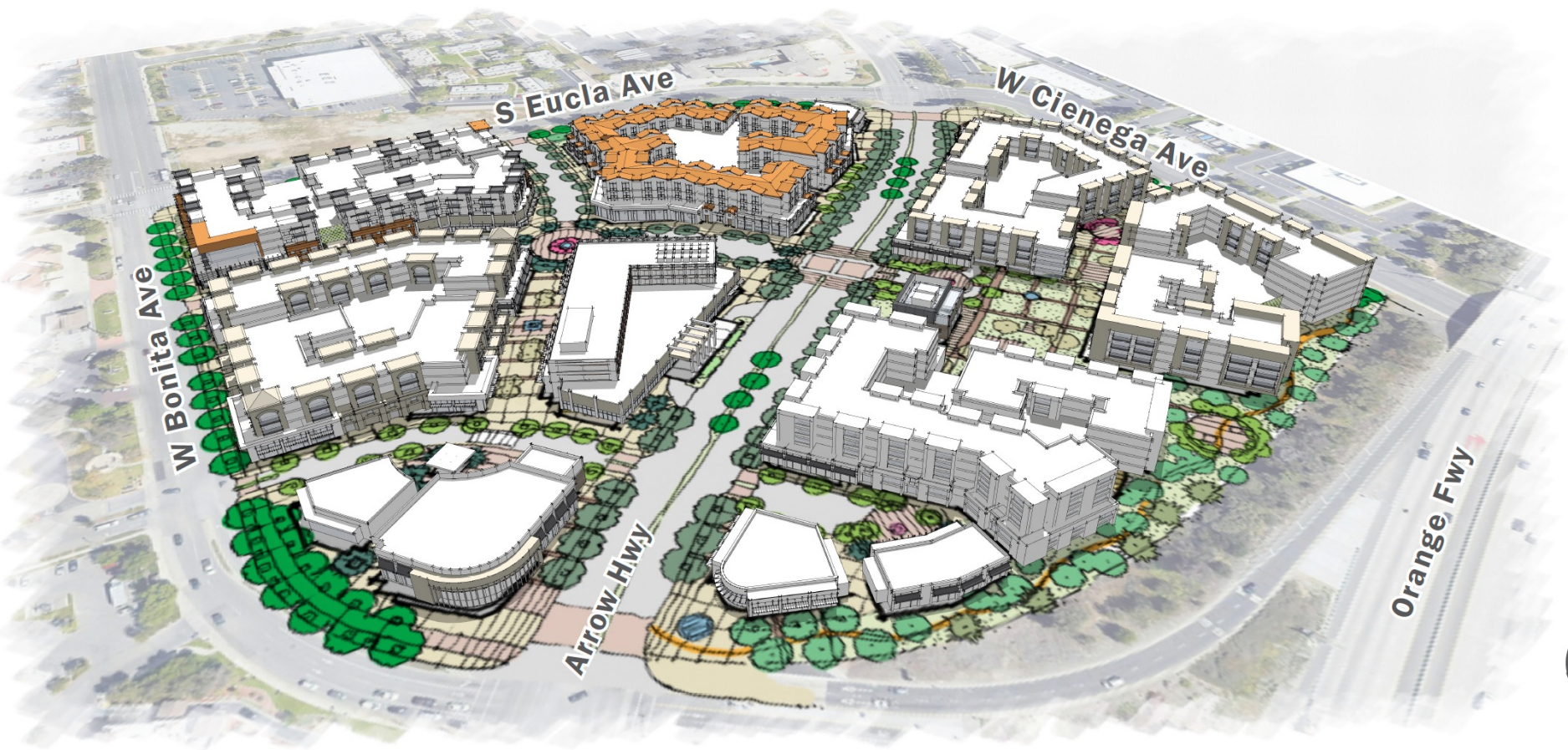
OPPORTUNITY SITES “MAIN STREET” CONCEPT PLAN RENDERING

PREFERRED OPTION: MAIN STREET



OPPORTUNITY SITES

Aerial View Looking East



OPPORTUNITY SITES

Aerial View Looking South



OPPORTUNITY SITES

➔ Arrow Highway Looking Northwest



OPPORTUNITY SITES

➔ Bonita Avenue Looking West



OPPORTUNITY SITES

➔ The Paseo (Plaza Space)



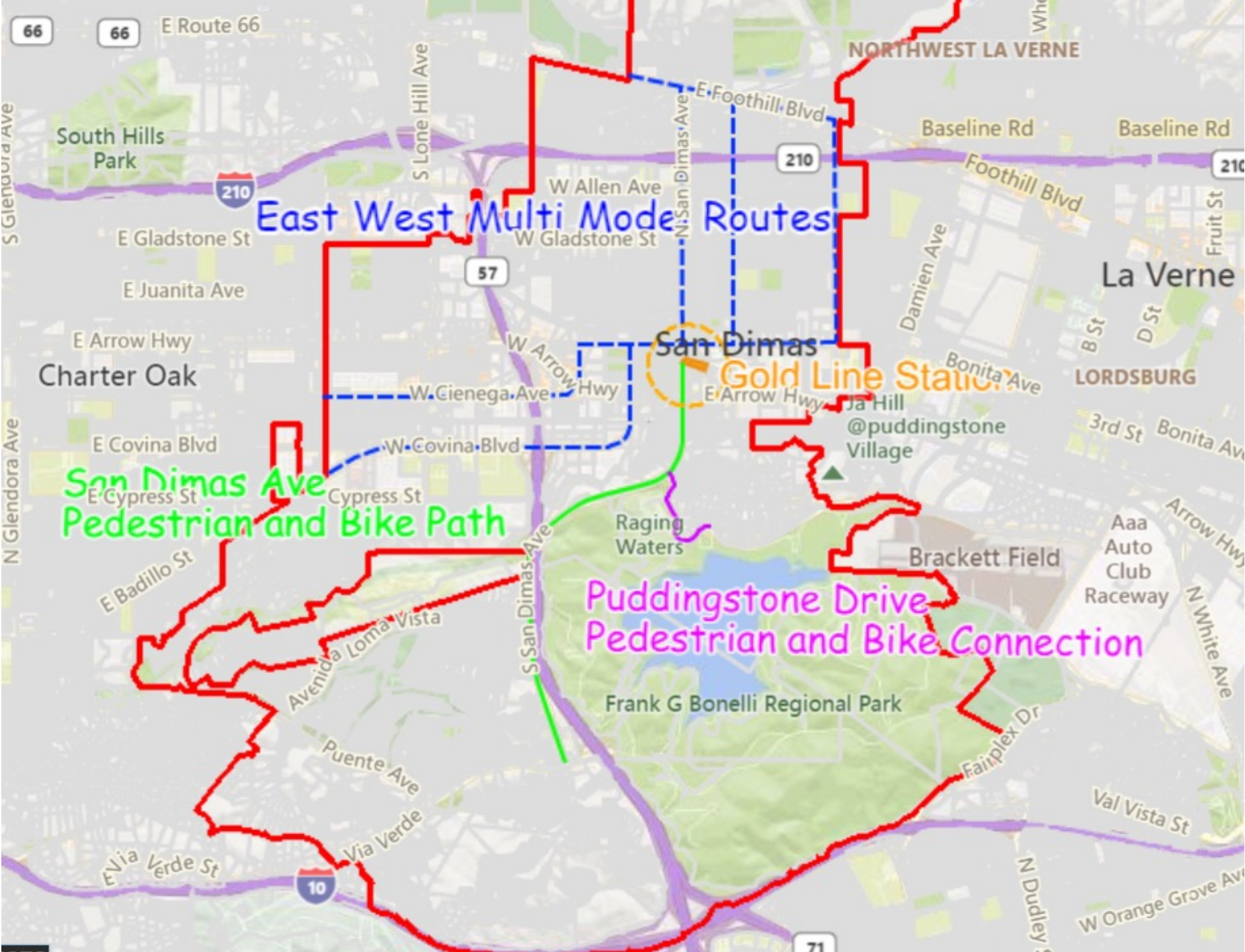
DRAFT MOBILITY STRATEGIES



MOBILITY STRATEGIES

- ➔ **Strategy 1:** Design a network of complete streets that facilitates safe, comfortable, and accessible connections between destinations for multiple modes of transportation while maintaining the small-town character of Downtown.
- ➔ **Strategy 2:** Improve connectivity to transit through the provision of high-quality bicycle and pedestrian infrastructure, streetscape improvements, wayfinding signage, and other first/last mile enhancements.
- ➔ **Strategy 3:** Design bicycle and pedestrian networks that are connected and prioritize the safety of all users. Emphasize increased access for users traveling via non-motorized modes.
- ➔ **Strategy 4:** Develop and incorporate parking management strategies that encourage efficient use of parking resources and support initiatives that can reduce parking demand needs.
- ➔ **Strategy 5:** Support transit-oriented land use development.





East West Multi Mode Routes

San Dimas Ave
Pedestrian and Bike Path

Puddingstone Drive
Pedestrian and Bike Connection

San Dimas
Gold Line Station

66

66

E Route 66

210

57

210

210

10

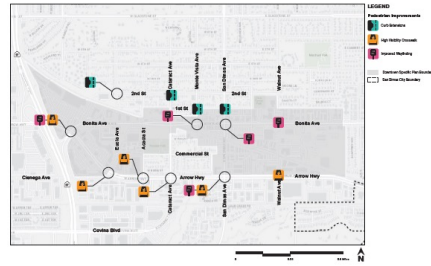
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WHERE WE NEED YOUR INPUT

Potential Bike Improvements



Potential Pedestrian Improvements



Potential Transit Improvements



Comments and Feedback

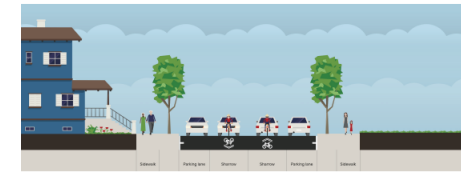


Potential Bikeway Treatments

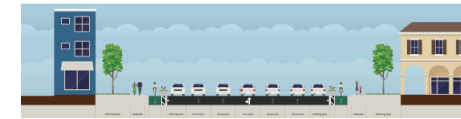
The figures below illustrate three examples of bikeway treatments that could be implemented on streets in the Downtown Specific Plan Area.



Class II Bike Lane Example
Class II Bike Lanes are on-street bicycle facilities marked by a painted line, without a vertical separation between the bike lane and vehicular traffic lane.



Class III Bike Route Example
Class III Bike Routes are on-street facilities that are shared with vehicles. These are typically placed on lower-volume streets and often marked with shared lane markings ("sharrows").



Class IV Separated Bikeway Example
Class IV Separated Bikeways are on-street facilities separated from adjacent vehicular traffic lanes by a vertical barrier, such as flexibile posts, bollards, planters, landscaped medians, or on-street parking.

Comments and Feedback



Maps of potential improvements for bicycle, pedestrian, and transit networks

Example street cross sections



STATIONS TO VISIT

- ➔ Land Use and Development/Design Standards
- ➔ Opportunity Sites Concepts
- ➔ Mobility Strategies

- ➔ We want to hear your comments and feedback tonight!

FOR MORE INFORMATION

- ➔ For questions or additional information, please contact:
 - ➔ Anne Nguyen, City of San Dimas at:
ANguyen@sandimasca.gov
 - ➔ Nick Pergakes, AICP, Interwest Consulting Group, at:
npergakes@interwestgrp.com





THANK YOU

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